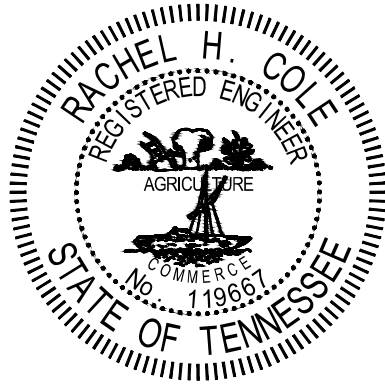


THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS.....	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E
TABULATED QUANTITIES	2F
PAVEMENT MARKING DETAIL.....	2G
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	T1

YEAR	PROJECT NO.	SHEET NO.
2025	NH/HSIP-29(128)	ROADWAY-SIGN1
	33S029-F8-007	
	33S029-F3-007	
<p align="center">STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION</p>		
<p align="center">SIGNATURE SHEET</p>		



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

John Hale

Contact Info: Rachel H. Cole, P.E.
<rachel.cole@arcadis.com>
Date: 2025.01.08 09:25:37-05'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED
AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE
ELECTRONIC DOCUMENTS.

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN2
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS.....	1A
ESTIMATED ROADWAY QUANTITIES	2
TABULATED QUANTITIES	2F

YEAR	PROJECT NO.	SHEET NO.
2025	NH/HSIP-29(128)	ROADWAY-SIGN2
	33S029-F8-007	
	33S029-F3-007	

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

HAMILTON COUNTY

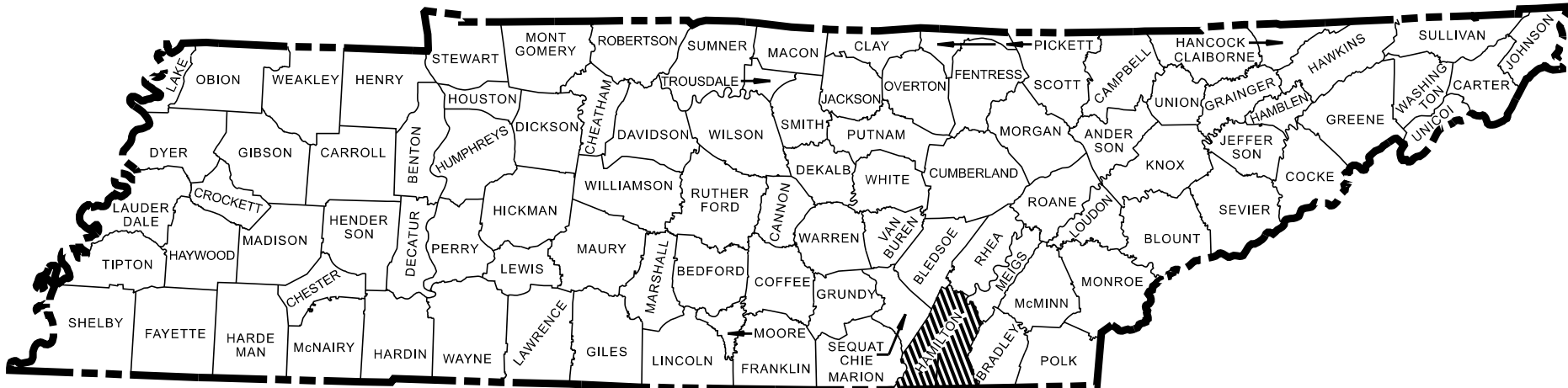
STATE ROUTE 29 (US-27)
FROM NEAR SR-111 (LM 20.76)
TO NEAR OLD DAYTON PIKE (LM 25.72)

RESURFACE & SAFETY
COLD PLANE, BRIDGE REPAIR, RESURFACE, AND PAVEMENT MARKINGS

STATE HIGHWAY NO. 29 F.A.H.S. NO. 27

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH/HSIP-29(128)	
STATE PROJ. NO.	33S029-F8-007; 33S029-F3-007	
BRIDGE	33S029-M3-006	



PROJECT LOCATION
BRIDGE ID. #33SR0290095, #33SR0290097, #33SR0290031, #33SR0290033,
#33SR0290034, #33SR0290035

NO EXCLUSIONS



33S029-F8-007
33S029-F3-007
END PROJECT NO. NH/HSIP-29(128) RESURFACE & SAFETY
LM 25.72

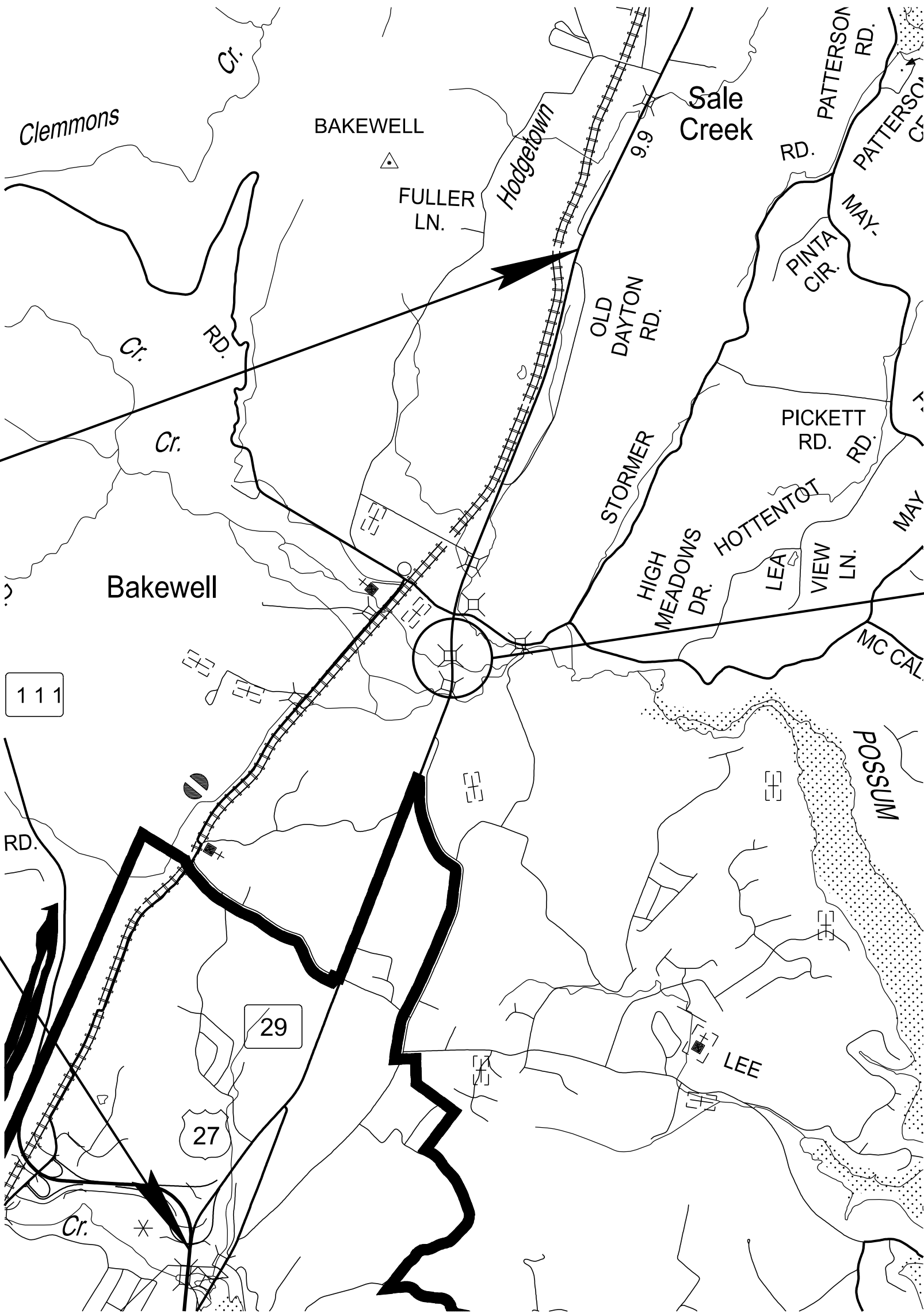
33S029-F8-007
33S029-F3-007
BEGIN PROJECT NO. NH/HSIP-29(128) RESURFACE & SAFETY
LM 20.76

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES
CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW
THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF
THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND
ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS
AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : ANDREW ZAZZARA, P.E.
DESIGNED BY : ARCADIS
DESIGNER : ADAM GRAVITT
P.E. NO. 98023-4217-04
PIN NO. 132851.00
CHECKED BY : RACHEL H. COLE, P.E.



PROJECT LENGTH 4.96 MILES
TOTAL LANE MILES RESURFACED 20.58 MILES

BIG POSSUM CREEK BRIDGE REPAIR
PROJECT NO. 33S029-M3-006
LM 23.71 LT & RT

APPROVED:
WILL REID, CHIEF ENGINEER

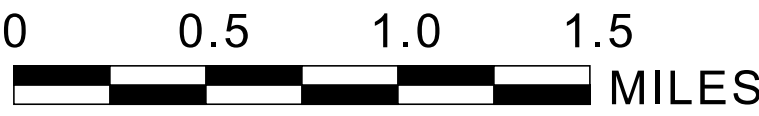
DATE:

APPROVED:
HOWARD H. ELEY, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED:
DIVISION ADMINISTRATOR DATE

SCALE: 1"= 2640'



TRAFFIC DATA	
ADT (2025)	17680
POSTED SPEED	
LM 20.76 - LM 21.32	60 MPH
LM 21.32 - LM 25.63	55 MPH
LM 25.63 - LM 25.72	45 MPH

ROADWAY INDEX

STANDARD ROADWAY DRAWINGS

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION
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SIGNATURE SHEET	ROADWAY-SIGN1
SIGNATURE SHEET	ROADWAY-SIGN2
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS.....	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES.....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E
TABULATED QUANTITIES	2F
PAVEMENT MARKING DETAIL.....	2G
UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	T1
BRIDGE PLANS.....	B1

NOTE: THE ALPHABETICAL LETTERS "I", "O" & "Q" ARE NOT USED IN NUMBERING OF SHEETS.

10-100.00 STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS

RD-TP-1	10-01-24	STANDARD ROADWAY DRAWINGS TITLE SHEET
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

10-104.00 ROADWAY, PAVEMENT APPURTENANCES, AND FENCES

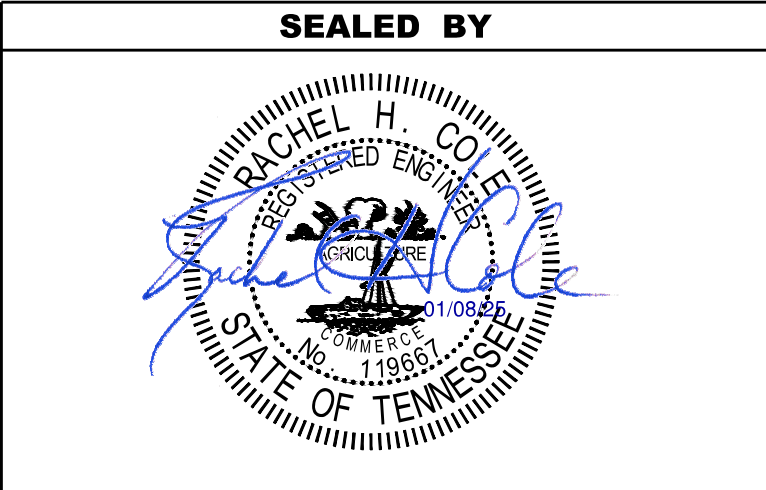
RP-J-23	01-28-22	CONCRETE PAVEMENT REPAIR DETAILS
RP-J-24	05-01-20	CONCRETE PAVEMENT SPALL AND RANDOM CRACK REPAIR DETAILS
RP-J-25	05-01-20	CONCRETE PAVEMENT JOINT REPAIR DETAILS

10-107.00 DESIGN - TRAFFIC CONTROL

T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-09-24	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	07-07-23	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	07-17-20	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-6	03-01-23	MARKING DETAIL FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-15	06-28-19	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES)
T-M-15A	06-28-19	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED RURAL ROUTES
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-29(128)	1A
		33S029-F8-007	
		33S029-F3-007	

REVISION - LETTING 01-08-25 : ADDED ROADWAY-SIGN2 TO INDEX

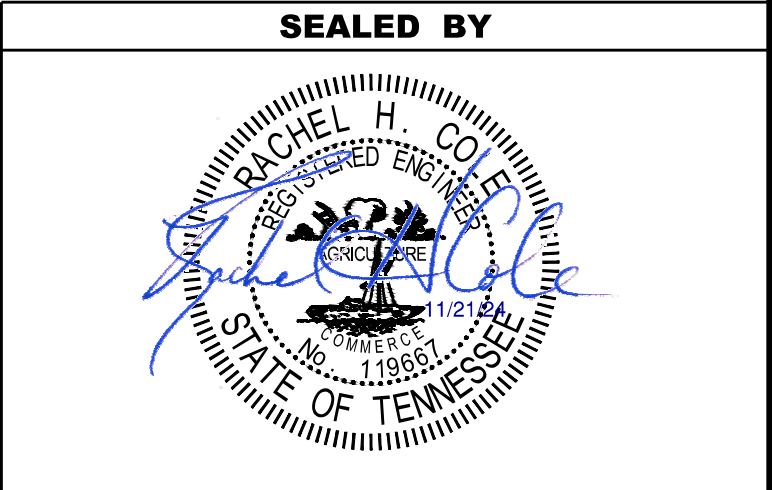


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-29(128)	1B
		33S029-F8-007	
		33S029-F3-007	

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	Environmental Division, Hazardous Materials	Asbestos Containing Material (ACM) surveys were completed on Bridge No. 33SR0290033 SR-29 NB over Big Possum Creek LM 23.71 (33-SR029-23.71R) and Bridge No. 33SR0290034 SR-29 SB over Big Possum Creek LM 23.71 (33-SR029-23.71L). No asbestos was detected. Please see the reports for further details and photographs. No special accommodations for demolition and waste disposal are anticipated for these structures and the material can be deposited in a C&D landfill. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03).	bridge



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

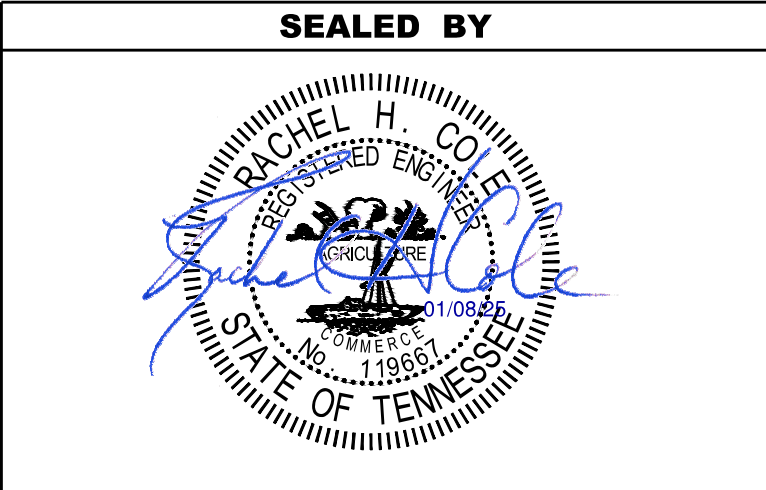
PROJECT
COMMITMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSSIP-29(128)	2
		33S029-F8-007	
		33S029-F3-007	

REVISION - LETTING 01-08-25 : CHANGED
QUANTITY OF ITEM NO. 503-01

ESTIMATED ROADWAY QUANTITIES						
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 33S029-F8-007	QUANTITY 33S029-F3-007	TOTAL QUANTITY	
(1)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	19.9		19.9	
(10)	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	2944		2944	
(2)	403-02.01 TRACKLESS TACK COAT	TON	102		102	
(12)	411-01.07 ACS MIX (PG64-22) GRADING E SHOULDER	TON	3867		3867	
(3)	411-01.21 LONGITUDINAL JOINT SEALANT	L.M.	21.3		21.3	
(4)	411-02.10 ACS MIX(PG70-22) GRADING D	TON	11620		11620	
	411-12.01 SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	9.5		9.5	
	411-12.02 SCORING SHOULDERS (NON-CONTINUOUS) (16IN WIDTH)	L.M.	9.9		9.9	
(5)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	15340		15340	
(11)	502-01 CLEANING AND SEALING JOINTS	L.F.	20300		20300	
(11)	502-08.07 SEALING RANDOM CRACKS (SILICONE SEALANT)	L.F.	2030		2030	
(11)	503-01 GRINDING CONCRETE PAVEMENT	S.Y.	11805		11805	
	712-01 TRAFFIC CONTROL	LS	1		1	
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	261		261	
(6)	712-06 SIGNS (CONSTRUCTION)	S.F.	1902		1902	
	712-08.03 ARROW BOARD (TYPE C)	EACH	2		2	
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	6		6	
	716-01.23 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	895		895	
(7)	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	895		895	
	716-02.04 PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.		242	242	
(8)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.		490	490	
(8)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH		26	26	
	716-02.07 PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	713		713	
(8)	716-03.04 PLASTIC WORD PAVEMENT MARKING (SCHOOL)	EACH		4	4	
(8)	716-04.14 PLASTIC PAVEMENT MARKING (LANE REDUCTION ARROW)	EACH		4	4	
(9)	716-05.01 PAINTED PAVEMENT MARKING (4" LINE)	L.M.	22.6		22.6	
(9)	716-05.05 PAINTED PAVEMENT MARKING (STOP LINE)	L.F.	490		490	
(11)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	2.65		2.65	
(11)	716-09.86 CONTRAST PAVEMENT MARKING 6"	L.M.		2.65	2.65	
(11)	716-09.90 CONTRAST PAVEMENT MARKING 6" DOTTED	L.F.		1740	1740	
(13)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	22.6		22.6	
	716-12.05 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	1020		1020	
	716-12.06 ENHANCED FLAT LINE THERMO (8IN LINE)	L.F.	2174		2174	
	717-01 MOBILIZATION	LS	1		1	
(14)	730-14.02 SAW SLOT	L.F.		3390	3390	
(14)	730-14.03 LOOP WIRE	L.F.		7250	7250	

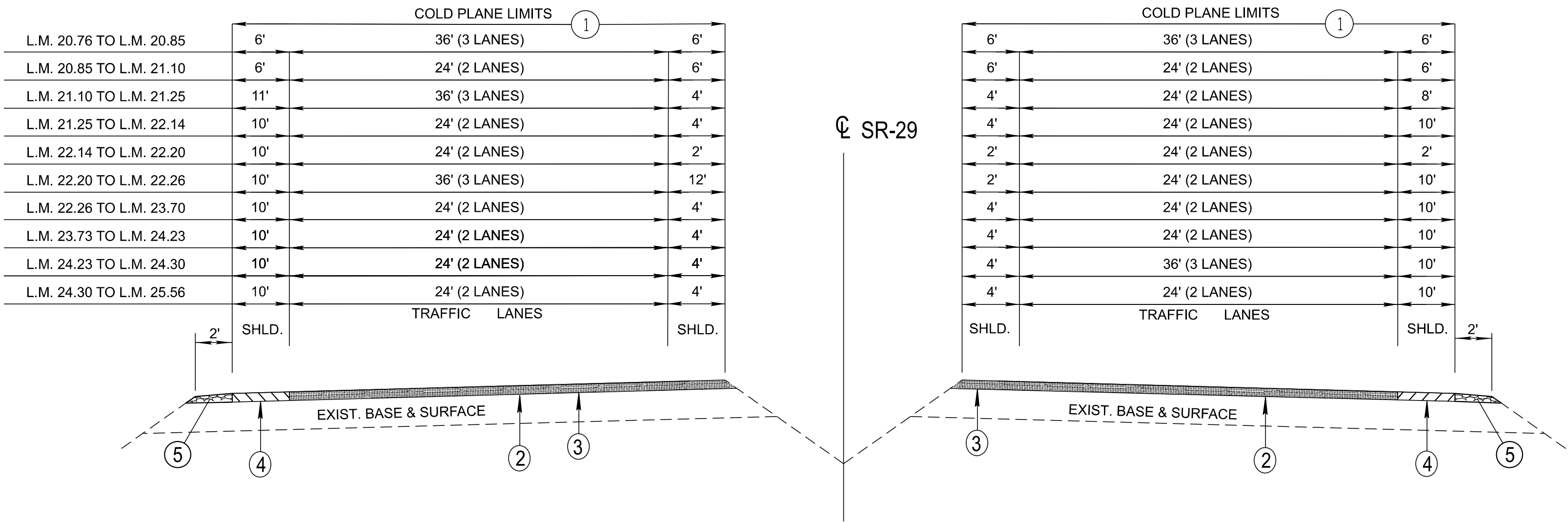
FOOTNOTES	
(1)	INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET 2D, SPECIAL PAVING NOTES (1) & (4).
(2)	INCLUDES 5 TONS FOR COUNTY ROADS, CROSS-OVERS, TURN LANES AND DRIVES.
(3)	USE CRAFCO PAVEMENT JOINT ADHESIVE #34524. PAVON JOINT ADHESIVE BY PAVON CORPORATION OR DENSO TAPE BY DENSO.
(4)	INCLUDES 603 TONS FOR BUSINESS ENTRANCES, COUNTY ROADS, CROSS-OVERS, TURN LANES, DRIVEWAYS, AND FIELD ENTRANCES.
(5)	INCLUDES 580 TONS FOR COUNTY ROADS AND CROSS-OVERS.
(6)	SEE SHEET NUMBER 2F FOR SIGNS (CONSTRUCTION) TABULATION BLOCK.
(7)	TO BECOME PROPERTY OF THE CONTRACTOR. INCLUDES ALL COST ASSOCIATED WITH PROPER DISPOSAL OF SNOWPLOWABLE MARKERS.
(8)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(9)	QUANTITY IN RESURFACING COLUMN IS FOR TEMPORARY MARKING ON MILLED SURFACE.
(10)	INCLUDES 351 TONS TO BE USED FOR RAMP REPAIR. SEE TABULATION ON SHEET 2F FOR INFORMATION.
(11)	FOR RAMP REPAIR WORK. SEE SHEET 2F FOR TABULATION.
(12)	INCLUDES 134 TONS FOR CROSS-OVERS WITHOUT TURN LANES.
(13)	CONTRACTOR SHALL USE THE EXTRUDED OR RIBBON METHOD FOR APPLICATION.
(14)	FOR REPLACING LOOPS AS DIRECTED BY THE TDOT PROJECT ENGINEER AT HIGHWATER RD (LM 22.192) AND MCCALLIE FERRY RD RT./ RETRO HUGHES RD LT (LM 23.947).



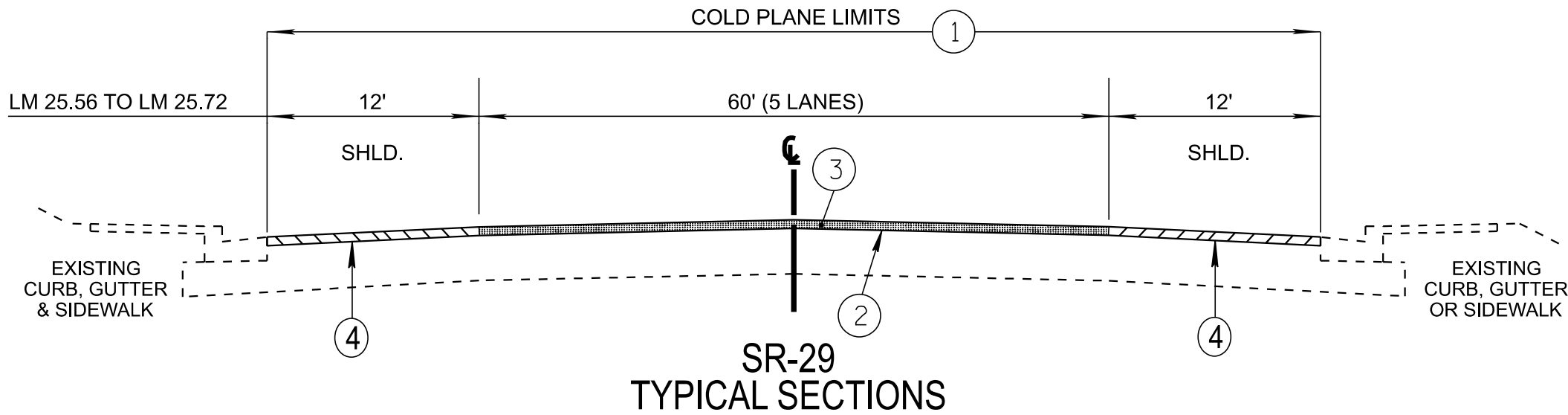
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

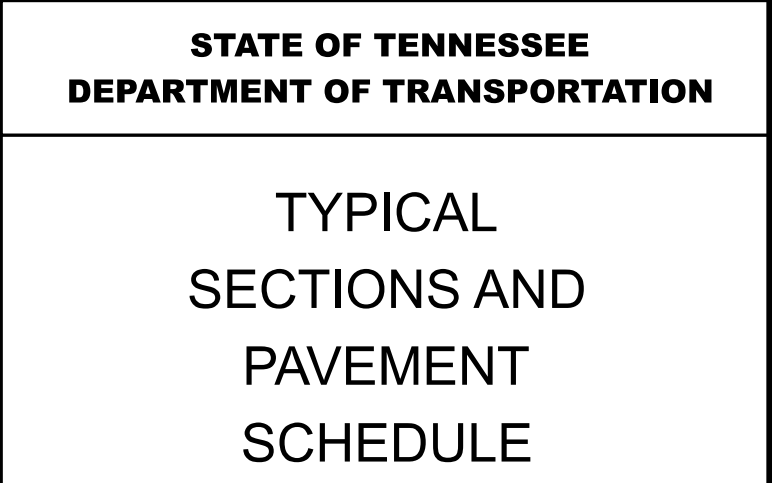
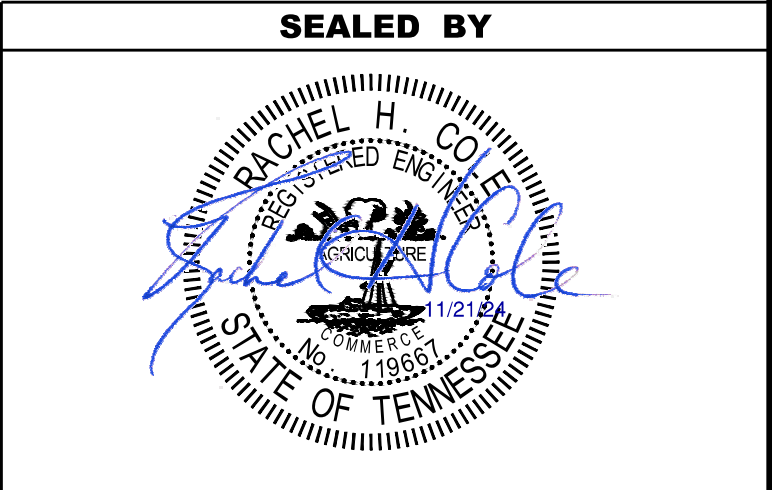
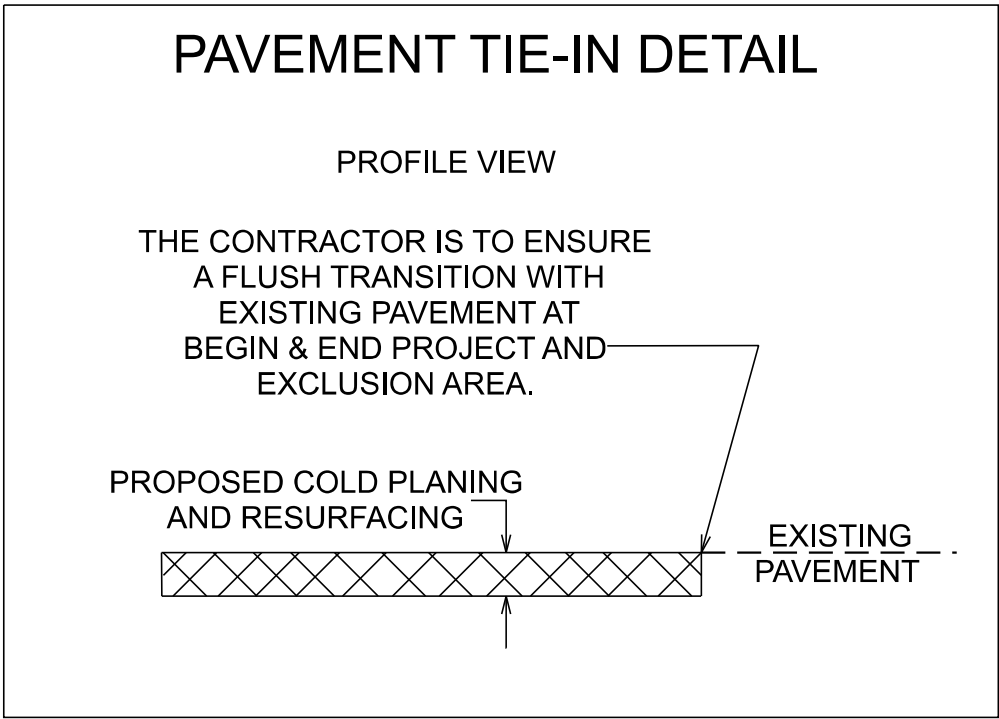
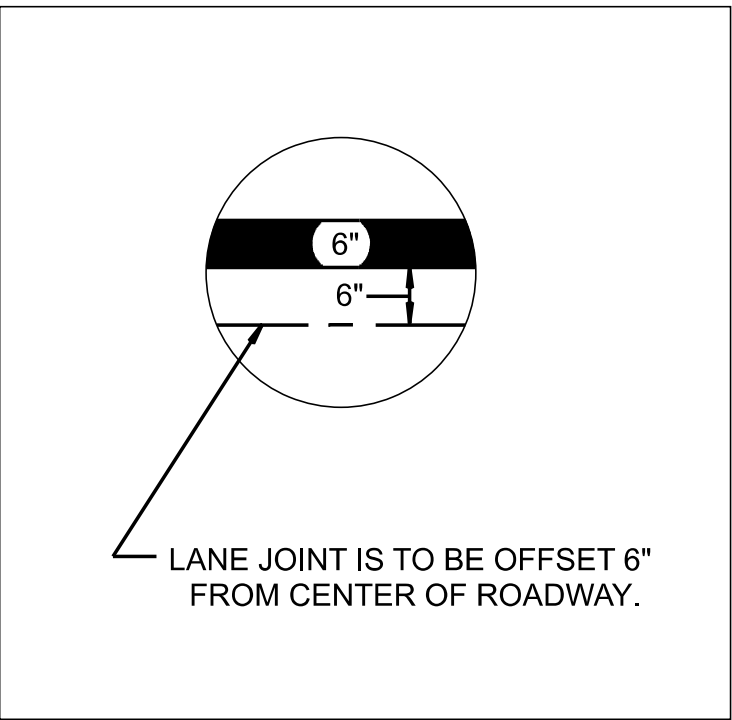
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-29(128)	2B
		33S029-F8-007	
		33S029-F3-007	



- A. LM 23.70 TO LM 23.73 (NORTHBOUND AND SOUTHBOUND) - EXCLUDE 149 LF OF MILLING / PAVING ON BIG POSSUM CREEK NB & SB BRIDGE AND APPROACHES (LM 23.70 TO LM 23.73). SEE BRIDGE REPAIR SHEETS FOR PROPOSED TREATMENT.
- B. LONGITUDINAL JOINT SEALANT, 411-01.21, SHALL BE USED ON LANE JOINTS.
- C. CROSSOVERS WITH TURN LANES (APPROX. 5650 SY) SHALL BE MILLED AND PAVED WITH GRADE "D" MIX.
- D. CROSSOVERS WITHOUT TURN LANES (APPROX. 2022 SY) SHALL BE MILLED AND PAVED WITH GRADE "E" MIX (SHOULDERS).



PROPOSED PAVEMENT SCHEDULE	
① COLD PLANING 1.25" THICK (APPROX. 131.25 LBS/SY) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT, TON	③ SURFACE MIX 1.25" THICK @ APPROX. 132.5 LBS/SY ITEM 411-02.10 ACS MIX (PG70-22) GRADING "D", TON
② TRACKLESS TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD. ITEM 403-02.01 TRACKLESS TACK COAT (TC), TON	④ PAVED SHOULDERS 1.25" THICK @ APPROX. 132.5 LBS/SY ITEM 411-01.07 ACS MIX (PG64-22) GRADING "E" SHOULDER, TON
⑤ STONE SHOULDERS 2" THICK @ 2.03 TONS/CY 303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D, TON	



GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (1) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01, PAINTED PAVEMENT MARKING (4" LINE), L.M.

FINAL PAVEMENT MARKING

- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMO INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

SIGNALIZATION

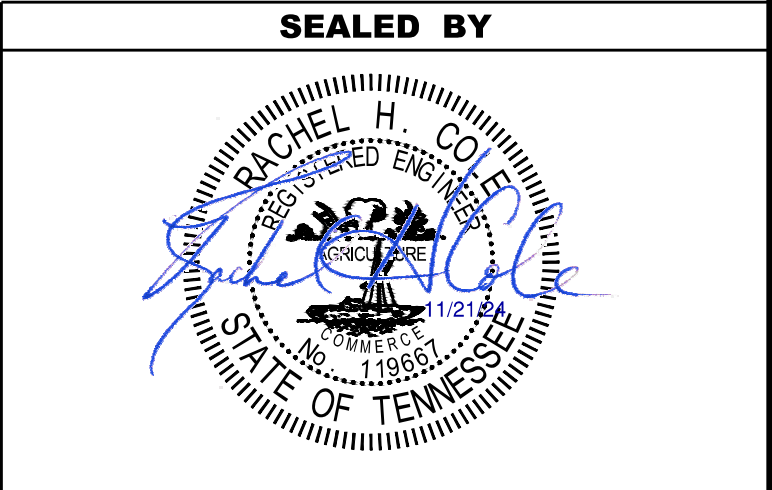
- (1) EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNALS SHALL COMPLY WITH TDOT STANDARD SPECIFICATIONS, SECTION 730.
- (6) SALVAGEABLE EQUIPMENT SHALL BECOME THE PROPERTY OF HAMILTON COUNTY AND SHALL BE STOCKPILED AT A LOCATION DESIGNATED BY THE ENGINEER FOR PICKUP BY HAMILTON COUNTY.
- (7) IF RESURFACING IS INCLUDED IN THE PROJECT, SIGNAL DETECTION LOOPS SHALL BE INSTALLED BEFORE THE FINAL SURFACE IS APPLIED.
- (11) THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE ON THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MAKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.
- (13) LOOPS SHALL BE INSTALLED IN THE LEVELING COURSE IF A LEVELING COURSE IS PROVIDED.
- (14) LOOP REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

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RESURF.	2025	NH/HSIP-29(128)	2C
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		33S029-F3-007	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

SPECIAL NOTES

TRAFFIC CONTROL

- (1)

THE CONTRACTOR SHALL MAKE PROVISIONS TO SAFELY CONTROL TRAFFIC INGRESS AT ALL ROADWAY ENTRANCES WITHIN THE FLAGGED WORK ZONE TO THE SATISFACTION OF THE TDOT PROJECT ENGINEER. ALL COST ASSOCIATED WITH THE CONTROLS WILL BE INCLUDED IN THE PRICE BID FOR TRAFFIC CONTROL.
- (2)

EXISTING CONSTRUCTION, REGULATORY AND WARNING SIGNS WHICH CONFLICT WITH THE CONSTRUCTION SIGNING SHALL BE REMOVED DURING CONSTRUCTION AND REINSTALLED AS DIRECTED BY THE TDOT PROJECT ENGINEER. ALL COSTS TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-01, TRAFFIC CONTROL, PER LUMP SUM.
- (3)

THE CONTRACTOR SHALL GIVE THE TDOT PROJECT ENGINEER A MINIMUM OF SEVEN (7) DAYS NOTICE PRIOR TO STARTING WORK SO THAT SUFFICIENT NOTICE CAN BE PREPARED AND DISTRIBUTED TO THE MEDIA.
- (4)

IMMEDIATELY UPON COMPLETION OF EACH CONSTRUCTION PHASE, ALL TRAFFIC CONTROL ITEMS THAT ARE NOT NECESSARY FOR THE SUCCEEDING PHASE SHALL BE REMOVED, COVERED, OR TURNED TO FACE AWAY FROM THE TRAFFIC.
- (5)

THE CONTRACTOR SHALL PROVIDE FLAGGER AT EACH SIDEROAD WITHIN THE PAVING OPERATION.
- (6)

A MINIMUM OF TWO (2) TYPE "C" ARROW BOARDS WILL BE REQUIRED, IF THE CONTRACTOR SCHEDULES HIS OPERATIONS SUCH THAT MORE THAN TWO (2) ARROW BOARDS ARE REQUIRED. THE COST OF ALL ADDITIONAL ARROW BOARDS WILL BE AT THE CONTRACTOR'S EXPENSE.
- (7)

CHANGEABLE MESSAGE SIGNS UNITS (ITEM NO. 713-16.01) WILL BE REQUIRED ON THIS PROJECT. THE SIGNS WILL BE USED FOR ADVANCED WARNING OR AS DIRECTED BY THE ENGINEER.
- (8)

MESSAGE BOARDS SHALL BE DISPLAYED A MINIMUM OF SEVEN (7) DAYS PRIOR TO STARTING WORK TO INFORM PUBLIC OF UPCOMING CONSTRUCTION PROJECT.
- (9)

PRIOR TO DISPLAYING MESSAGES ON CHANGEABLE MESSAGE BOARDS, ALL MESSAGES SHALL BE APPROVED BY THE TDOT PROJECT ENGINEER AND REGION 2 TRAFFIC ENGINEER.
- (10)

INFORMATION ON CHANGEABLE MESSAGE BOARDS SHALL BE UPDATED TO REFLECT CURRENT CONSTRUCTION CONDITIONS AND ACTIVITIES AT ALL TIMES.

LANE CLOSURES

- (1)

THE RESTRICTION OF TRAFFIC TO ONE LANE SHALL NOT EXCEED ONE DAY'S PAVING OPERATION.
- (2)

THE CONTRACTOR SHALL KEEP ALL TRAFFIC LANES OPEN TO TRAFFIC DURING NON-WORKING HOURS AND/OR NON-WORK DAYS.
- (3)

OVER-NIGHT LANE CLOSURES WILL NOT BE ALLOWED UNLESS DIRECTED BY THE TDOT PROJECT ENGINEER.
- (4)

THE CONTRACTOR SHALL NOT BE ALLOWED TO INTERRUPT TRAFFIC FLOW AND SHALL MAINTAIN ALL LANES OF TRAFFIC IN EACH DIRECTION ON THE FOLLOWING DAYS:

A.

OFFICIAL STATE HOLIDAYS.

B.

FRIDAY AT 6:00 AM UNTIL TUESDAY AT 7:00 PM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON MONDAY.

C.

THURSDAY AT 6:00 AM UNTIL SUNDAY AT 7:00PM, IF A STATE HOLIDAY OCCURS OR IS OBSERVED ON FRIDAY.

D.

DURING LOCAL FESTIVALS, GAMES OR EVENTS THAT COULD BE IMPEDED BY THE PAVING OPERATIONS WHERE AND AS DIRECTED BY THE TDOT PROJECT ENGINEER.

PAVING

- (1)

CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

•

BROOMING & DEGRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OR ROADWAY AND SHOULDERS AS DIRECTED BY THE TDOT PROJECT ENGINEER.

•

ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED.

•

ALL COSTS ASSOCIATED WITH CLIPPING, BROOMING AND DE-GRASSING SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
- (2)

INTELLIGENT COMPACTION SHALL BE USED.
- (3)

ANY QUANTITY REMAINING ON ITEMS COMPLETED PRIOR TO THE PAVING OPERATION WILL NOT BE CONVERTED TO ADDITIONAL ASPHALT FOR THE ROADWAY.
- (4)

THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK.

•

REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 208-01.05.

MISCELLANEOUS

- (1)

ALL SAFETY IMPROVEMENTS TO BE PAID FOR UNDER PROJECT NUMBER: NH/HSIP-29(128); 33S029-F3-007.

PAVEMENT MARKING

- (1)

THE CONTRACTOR IS RESPONSIBLE FOR THE LAYOUT OF ALL PAVEMENT MARKING. ANY CHANGE FROM THE EXISTING PAVEMENT MARKINGS MUST BE APPROVED BY THE REGION2 TRAFFIC MANAGER.
- (2)

EXISTING PAVEMENT MARKING IN THE SIDE ROAD RADII SHALL BE RE-TRACED AS DIRECTED BY THE TDOT PROJECT ENGINEER DURING FINAL STRIPING.

JOINT SEALANTS

- (1)

THE CONTACT SURFACE OF LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY SPRAYING THE JOINT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING TH THIN, UNIFORM COAT.
- (2)

PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER
- (3)

TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT OT PREVENT DAMAGE FROM TRACKING.

COLD PLANING

- (1)

THE CONTRACTOR WILL BE REQUIRED TO PAVE THE COLD PLANED PAVEMENT WITHIN A PERIOD OF 96 HOURS.
- (2)

PROVIDED THAT THE CONTRACTOR USES A COLD PLANING MACHINE WITH THE MILLING DRUM SPECIFICATIONS OF CLOSELY SPACED FINE TOOTH (5/16 INCHES OR SMALLER) MILLING DRUM, MOVING FORWARD AT MODERATE SPEED (50 TO 65 FPM.) AND THE UNDER SURFACE REMAINS INTACT (HAVING NO LOOSE MATERIAL OR GOUGES,) THEN THE CONTRACTOR MAY PERMISSION FROM THE REGION 2 PROJECT ENGINEER TO MILL ALL SURFACES BEFORE COVERING WITH BITUMINOUS MATERIAL.
- (3)

IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE TDOT PROJECT ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.
- (4)

IF SUBSURFACE PAVEMENT FAILURE IS EXPOSED AFTER MILLING, REMOVE AREA OF FAILURE TO SOUND PAVEMENT AND REPLACE WITH "D" MIX PRIOR TO PLACING PROPOSED OVERLAY.

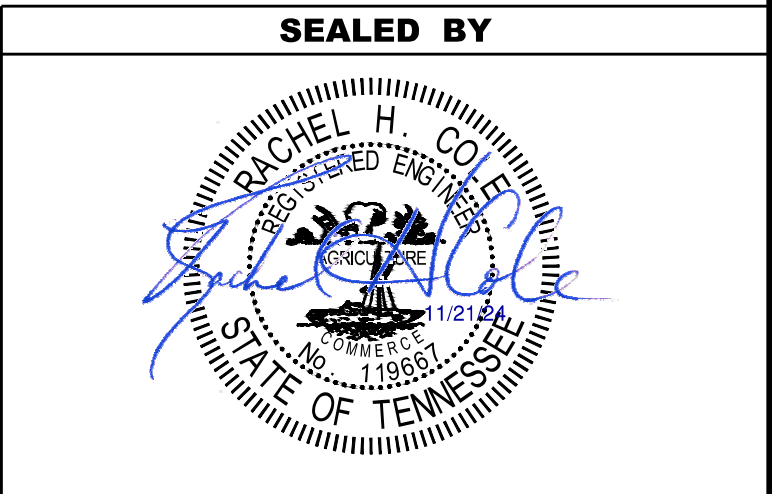
- (5)

AT THE COMPLETION OF COLD PLANING OPERATION, ANY DEPRESSIONS WILL BE CLEANED AND BROUGHT TO PROPER GRADE WITH "D" MIX MATERIAL. THE COST OF THE CLEAN UP WILL BE INCLUDED IN THE UNIT PRICE FOR "D" MIX MATERIAL.

- (6)

THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT NEAR EXISTING MANHOLES AS NOT TO DAMAGE MANHOLE OR CONCRETE RING AROUND EXISTING MANHOLE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE TDOT PROJECT ENGINEER AT NO ADDITIONAL COST.

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RESURF.	2025	NH/HSIP-29(128)	2D
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		33S029-F3-007	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4)

THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11)

SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12)

IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15)

IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

SCOPE OF WORK

- (6)

COLD PLANE, BRIDGE REPAIR, RESURFACE, AND PAVEMENT MARKINGS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1)

IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6)

EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31)

CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32)

WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33)

IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34)

ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35)

WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36)

ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37)

ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38)

OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39)

DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40)

WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44)

ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45)

FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46)

APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47)

ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48)

THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49)

IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50)

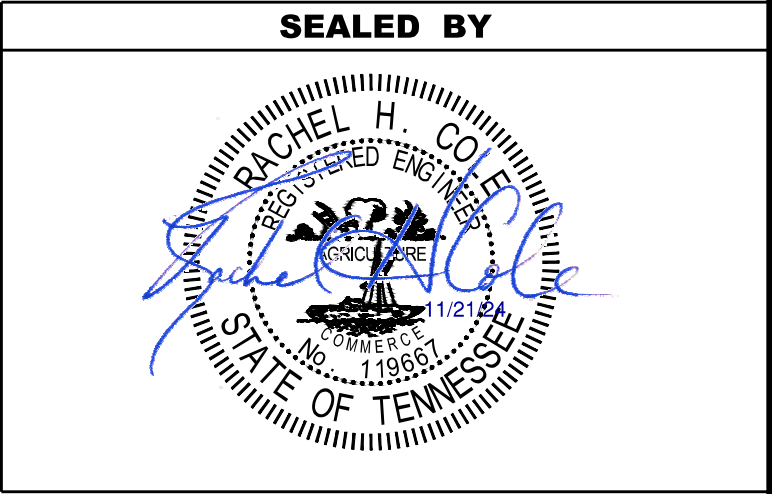
FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51)

IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52)

WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53)

CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-29(128)	2E
		33S029-F8-007	
		33S029-F3-007	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES			S.F.	TOTAL NUMBER REQUIRED
		L	x	W		
G20-1	ROAD WORK NEXT 5 MILES	48"	x	24"	8	3
G20-2	END ROAD WORK	48"	x	24"	8	21
W3-4	BE PREPARED TO STOP	48"	x	48"	16	2
W8-11	UNEVEN LANES	48"	x	48"	16	26
W8-15	GROOVED PAVEMENT	48"	x	48"	16	26
W8-15P	MOTORCYCLE (PLAQUE)	24"	x	18"	3	26
W20-1	ROAD WORK AHEAD	48"	x	48"	16	21
W20-1	ROAD WORK 1 MILE	48"	x	48"	16	3
W20-1	ROAD WORK 1/2 MILE	48"	x	48"	16	3
W20-1	ROAD WORK 1000 FT	48"	x	48"	16	3
W20-4	ONE LANE ROAD AHEAD	48"	x	48"	16	2
W20-5L	LEFT LANE CLOSED AHEAD	48"	x	48"	16	4
W20-5R	RIGHT LANE CLOSED AHEAD	48"	x	48"	16	4
W20-7	FLAGGER (SYMBOL)	48"	x	48"	16	4
W21-2	FRESH OIL	48"	x	48"	16	4
TOTAL						1902

STORM DRAIN ADJUSTMENTS (RESURFACING)
NO STORM DRAIN ADJUSTMENTS WITHIN PROJECT LIMITS

PROPOSED GUARDRAIL (RESURFACING)
NO PROPOSED GUARDRAIL ADJUSTMENTS WITHIN PROJECT LIMITS

UTILITY ADJUSTMENTS (RESURFACING)
NO UTILITY ADJUSTMENTS WITHIN PROJECT LIMITS

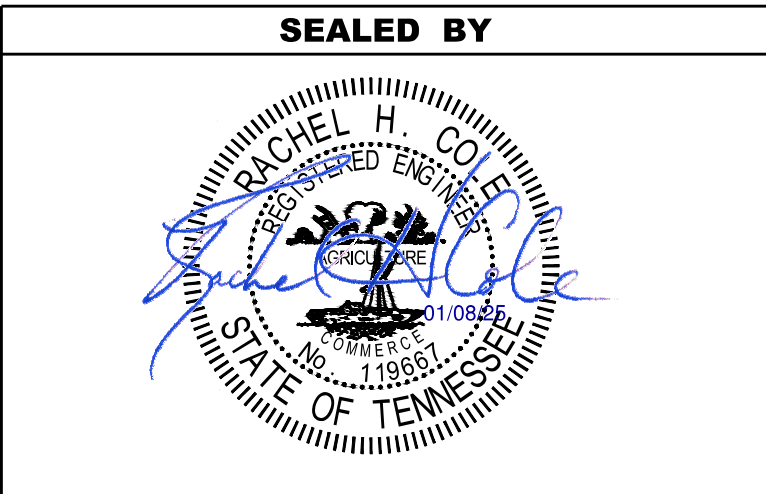
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-29(128)	2F
		33S029-F8-007	
		33S029-F3-007	

REVISION - LETTING 01-08-25 : CHANGED
QUANTITY OF ITEM NO. 503-01

BRIDGE DECK RECOMMENDATIONS (RESURFACING)					
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	VERTICAL CLEARANCE	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
33SR0290095	20.80	RAMP FROM SR-111 SB LANES	NBL 16'-8" SBL 16'-9"	N/A	MAINTAIN MINIMUM VERTICAL CLEARANCE, COLD PLANE TO MATCH THICKNESS OF TREATMENT.
33SR0290097	20.91	SR-111 LEFT LANES ONLY	SBL 16'-11"	N/A	MAINTAIN MINIMUM VERTICAL CLEARANCE, COLD PLANE TO MATCH THICKNESS OF TREATMENT.
33SR0290031	23.64 R	BRANCH	N/A	48.9'	PAVE WITH PLANS
33SR0290031	23.64 L	BRANCH	N/A	48.9'	PAVE WITH PLANS
33SR0290033	23.71 R	BIG POSSUM CREEK	N/A	100'-0"	PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY
33SR0290034	23.71 L	BIG POSSUM CREEK	N/A	100'-0"	PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY
33SR0290035	24.14 R	BRANCH	N/A	22'-0"	PAVE WITH PLANS
33SR0290035	24.14 L	BRANCH	N/A	22'-0"	PAVE WITH PLANS

SR-29 / SR-111 INTERCHANGE RAMP REPAIR WORK						
RAMP LOCATION	303-01 STONE TON	502-01 CLEAN/SEAL JOINTS L.F.	502-08.07 RANDOM CRACKS (SILICONE SEALANT) L.F.	503-01 GRINDING CONC. PVMT. S.Y.	716-05.20 PAINT PVMT. MRKG. (6" LINE) L.M.	716-09.86 CONTRAST PVMT. MRKG. (6" LINE) L.M.
FROM SR-111 SB TO SR-29 NB	65	12760	1276	7804	1.67	1.67
FROM SR-29 SB TO SR-111 NB	286	7540	754	4001	0.98	0.98
TOTALS	351	20300	2030	11805	2.65	2.65

MEASUREMENT FOR THE RAMPS STOPPED AT THE EDGE OF THE GRASS ON THE GORE AREA ON SR-111.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-29(128)	2G
		33S029-F8-007	
		33S029-F3-007	

SEALED BY

COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.0000 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 18M.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
DETAIL

SCALE: 1" = 50'

UTILITY

- (1)

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER "THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT", THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800- 351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3)

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

CABLE:
COMCAST (XFINITY)
2030 EAST POLYMER DRIVE
CHATTANOOGA, TN 37421
CONTACT: TIM GREGORY
OFFICE PHONE: (706) 252-4185
Email: tim_gregory@comcast.com

ELECTRIC:
ELECTRIC POWER BOARD OF CHATTANOOGA
PO BOX 182255
CHATTANOOGA, TN 37422
CONTACT: ADAM NORWOOD
OFFICE PHONE: (423) 483-3305
Email: norwoodaw@epb.net

ELECTRIC:
TENNESSEE VALLEY AUTHORITY
1101 MARKET ST MR-4G
CHATTANOOGA, TN 37402
CONTACT: STEPHEN WILLIAMS
OFFICE PHONE: (865) 632-2101
Email: sewilliams@tva.gov

FIBER:
ELECTRIC POWER BOARD OF CHATTANOOGA
PO BOX 182255
CHATTANOOGA, TN 37422
CONTACT: ADAM NORWOOD
OFFICE PHONE: (426) 483-3305
Email: norwoodaw@epb.net

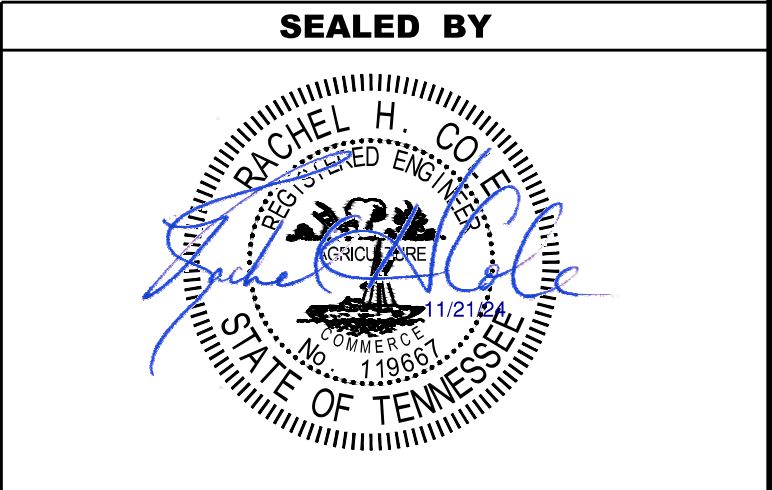
GAS:
MIDDLE TENNESSEE NATURAL GAS
PO BOX 670
SMITHVILLE, TN 37166
CONTACT: MATT STENNETT
OFFICE PHONE: (931) 754-3515
Email: mstennett@mtng.com

SEWER:
HAMILTON COUNTY WWTA
1250 MARKET STREET, SUITE 3050
CHATTANOOGA, TN 37402-2713
CONTACT: MICHAEL PATRICK
OFFICE PHONE: (423) 209-7842
Email: WWTAEng@HamiltonTN.org; Mpatrick@HamiltonTN.org

TELEPHONE:
BELLSOUTH dba AT&T
300 E MARTIN LUTHER KING BOULEVARD
CHATTANOOGA, TN 37403
CONTACT: JOE PERREL
OFFICE PHONE: (423) 266-1566
Email: Jp1389@att.com

WATER:
UNION FORK BAKEWELL UTILITY DISTRICT
405 RETRO HUGHES ROAD
BAKEWELL, TN 37304
CONTACT: DENISE KEYLON
OFFICE PHONE: (423) 332-9733
Email: denise@ufbud.com

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-29(128)	3
		33S029-F8-007	
		33S029-F3-007	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
AND
UTILITY OWNERS

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
- a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
- a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

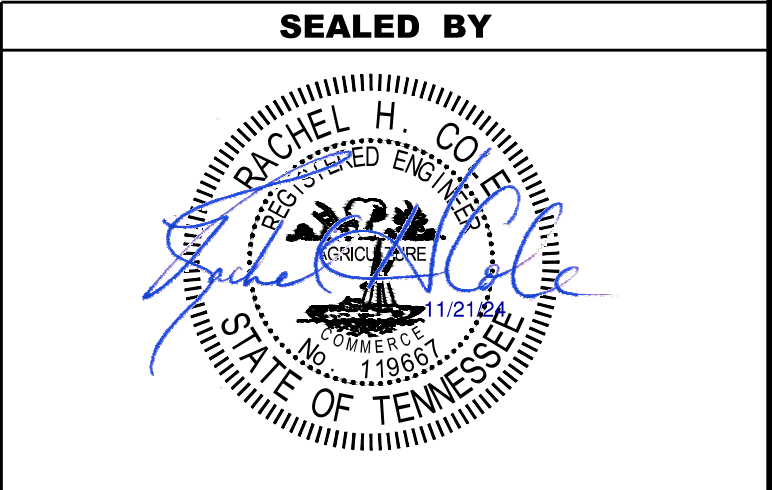
WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- b.

IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a. PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- c.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-29(128)	T1
		33S029-F8-007	
		33S029-F3-007	



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL



SHEET NAME	SHEET NO.
SIGNATURE SHEET _____	STRUCTURE-SIGN 1
BRIDGE PLANS _____	B1 THRU B7

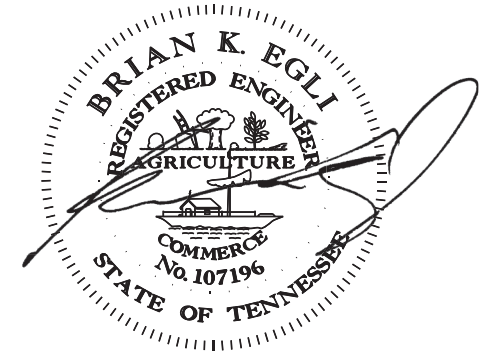
YEAR	PROJECT NO.	SHEET NO.
2025	33S029-M3-006	STRUCTURE-SIGN 1
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION		
SIGNATURE SHEET		

PIN 132851.00

[illegible]

<u>INDEX OF DRAWINGS</u>		DWG. NO.	LAST REV. DATE
SIGNATURE SHEET	-----	STRUCTURE-SIGN 1	
INDEX OF DRAWINGS	-----	B1	
BRIDGE TABULATION AND ESTIMATED QUANTITIES	-----	B2	
TYPE I THIN EPOXY OVERLAY NOTES	-----	B3	
APPROACH SLAB REPAIR DETAIL NOTES	-----	B4	
PLAN VIEWS (33SR0290033 & 33SR0290034)	-----	B5	
PHASE CONSTRUCTION (33SR0290033)	-----	B6	
PHASE CONSTRUCTION (33SR0290034)	-----	B7	

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 6/24
SUPERVISED BY K. MARTINKO DATE 6/24
CHECKED BY _____ DATE _____



11/07/24

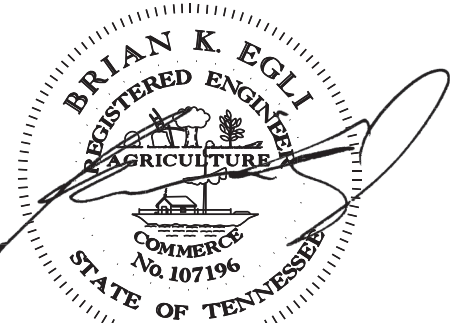
STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION
INDEX OF DRAWINGS
33-SR29-23.71R & L
OVER
BIG POSSUM CREEK
BR. NOS. 33SR0290033
33SR0290034
HAMILTON COUNTY
2025

PIN 132851.00

PROJECT NO.		YEAR	SHEET NO.
33S029-M3-006		2025	B2
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES				
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.53 CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	617-04.01 TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
33-SR29-23.71R OVER BIG POSSUM CREEK (33SR0290033)	M-136-110 M-136-113	CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	6	671
33-SR29-23.71L OVER BIG POSSUM CREEK (33SR0290034)	M-136-110 M-136-113	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)		671
TOTAL			6	1342



11/07/24
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE TABULATION AND
ESTIMATED QUANTITIES
33-SR29-23.71R & L
OVER
BIG POSSUM CREEK
BR. NOS. 33SR0290033
33SR0290034
HAMILTON COUNTY
2025

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 6/24
SUPERVISED BY K. MARTINKO DATE 6/24
CHECKED BY _____ DATE _____

PIN 132851.00

[illegible]

TYPE 1 THIN EPOXY OVERLAY NOTES :

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.

B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.

C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.

D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.

E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.

F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

<u>SIEVE SIZE</u>	<u>% PASSING</u>
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE WITH THE ENGINEER, MAY SUSPEND AN ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), SY.

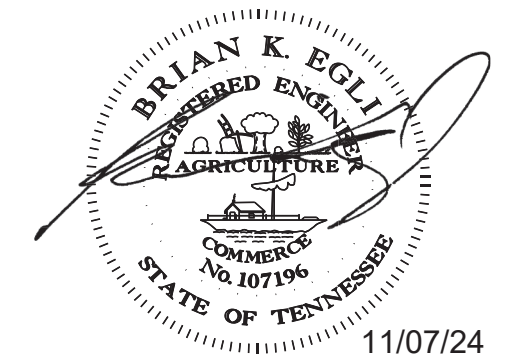
THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHAL BE PERFORMED IN ADJOINING AREAS, SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

**** SPECIAL NOTE:**

THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACKCOAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY.

MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACKCOAT OR DEBRIS REMOVAL.

REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.



11/07/24

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION
TYPE I THIN EPOXY
OVERLAY NOTES
33-SR29-23.71R & L
OVER
BIG POSSUM CREEK
BR. NOS. 33SR0290033
33SR0290034
HAMILTON COUNTY
2025

B3

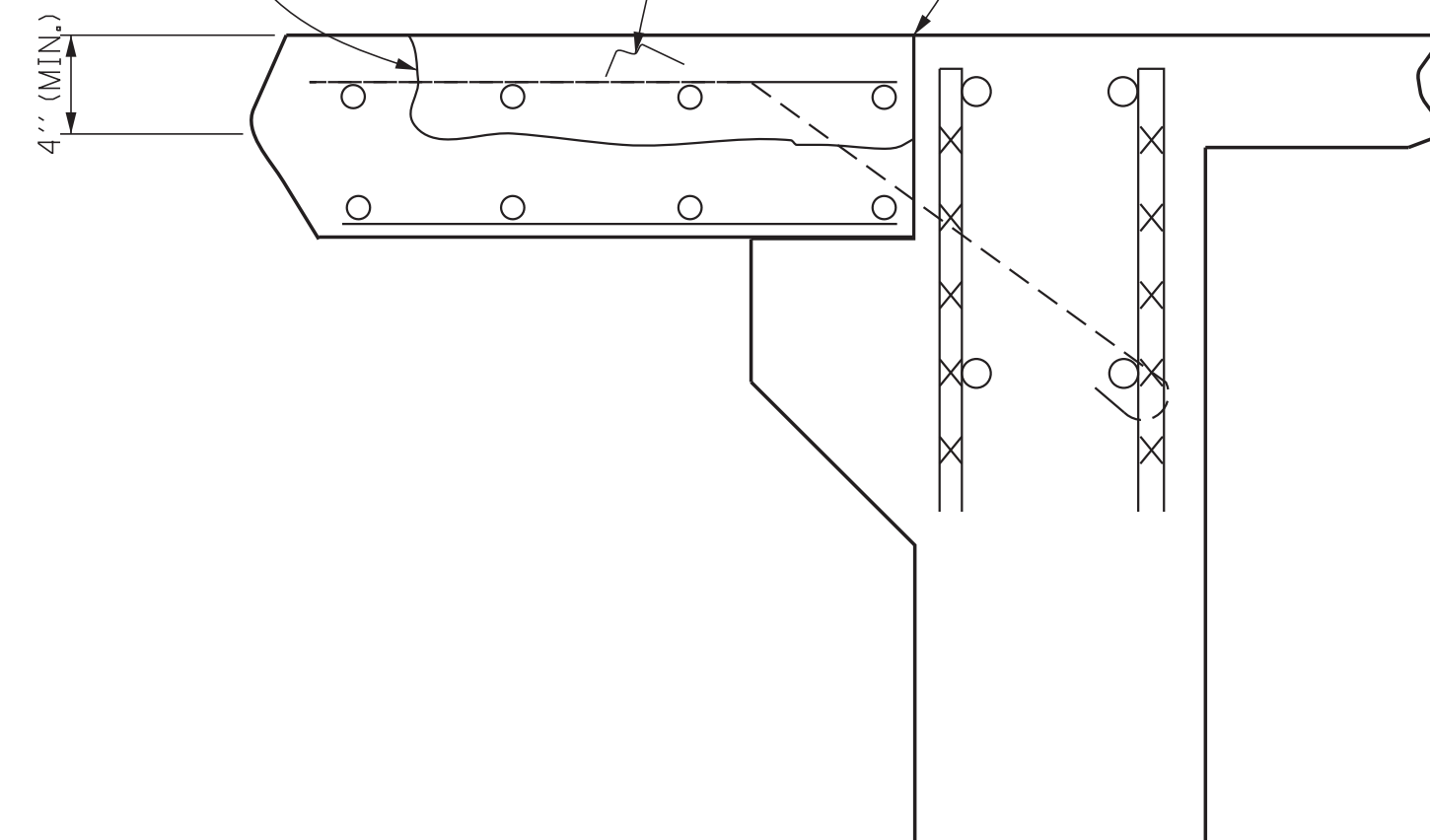
PIN 132851.00

[illegible]

USE AN EXTENDED RAPID SET CEMENTITIOUS PATCHING MATERIALS
(FOR PATCHING MATERIAL REFER TDOT QUALIFIED
PRODUCT LIST NO.13.004)

— INSTALL BOND BREAKER BEFORE PLACING CONCRETE FOR APPROACH SLAB REPAIR.

EDGES TO BE SAW CUT
TO A DEPTH OF 1"



CONCRETE APPROACH PAVEMENT REPAIR DETAILS (STRUCTURAL REPAIR)

NOTE: REMOVE CONCRETE TO A DEPTH OF $\frac{3}{4}$ " MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR (S.Y.).

NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT.)
THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED
BY THE ENGINEER.

POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE
IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE
FOLLOWING RESTRICTIONS:

(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS
HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. TRAFFIC
CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.

(2) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

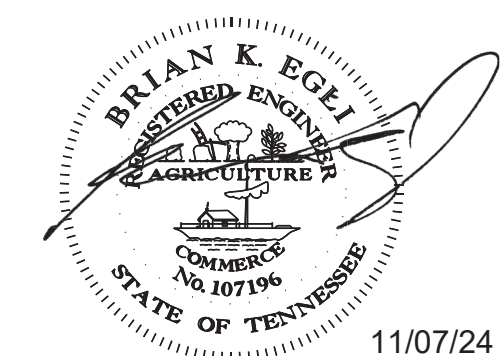
BRIDGE DECK REPAIR MATERIAL:

REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHATE PRODUCT FROM THE TDOT QUALIFIED PRODUCTS LIST:
13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS.
MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

MIX MUST MEET 3000 PSI BEFORE OPENING TO TRAFFIC.

3:29:35 AM
\$\$\$\$SYTIME\$\$\$\$

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 6/24
 SUPERVISED BY K. MARTINKO DATE 6/24
 CHECKED BY _____ DATE _____



11/07/24

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION
APPROACH SLAB REPAIR
DETAIL NOTES
33-SR29-23.71R & L
OVER
BIG POSSUM CREEK
BR. NOS. 33SR0290033
33SR0290034
HAMILTON COUNTY
2025

PROJECT NO.	YEAR	SHEET NO.
33S029-M3-006	2025	B5

[illegible]

149'-0"

(LIMITS OF TYPE I THIN EPOXY OVERLAY)

LIMITS OF BRIDGE DECK (100'-0")

LIMITS OF APPROACH SLAB (24'-6")

LIMITS OF APPROACH SLAB (24'-6")

APPROACH PARTIAL REPAIR

APPROACH PARTIAL REPAIR

SHLD. 5'-3"

LANE 12'-0"

LANE 12'-0"

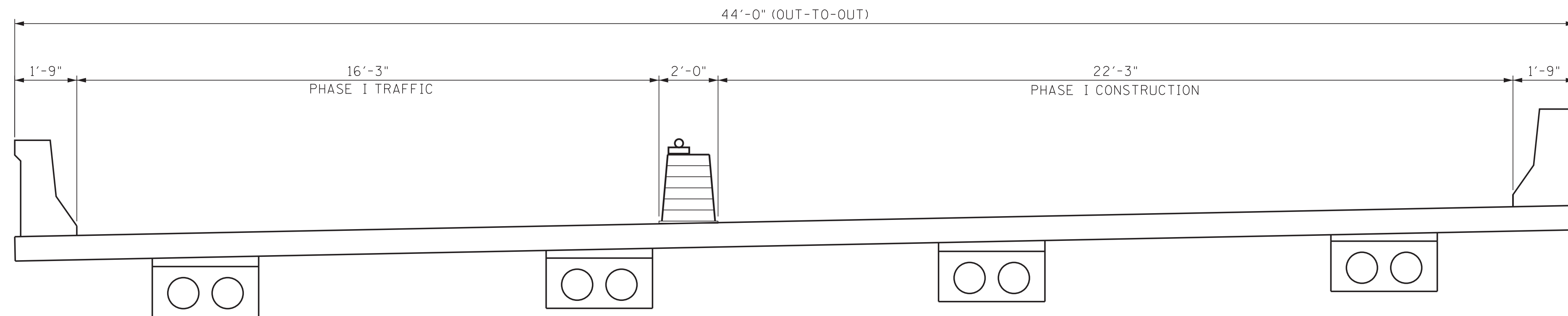
SHLD. 11'-3"

BRIDGE ID. NO. 33SR0290033



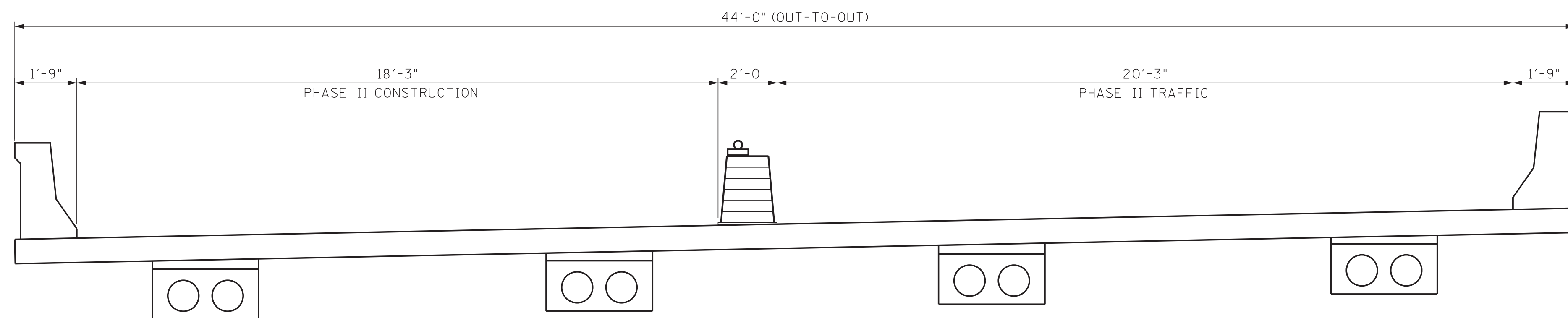
B5

PROJECT NO.	YEAR	SHEET NO.
33S029-M3-006	2025	B6

[illegible]

PHASE I CONSTRUCTION

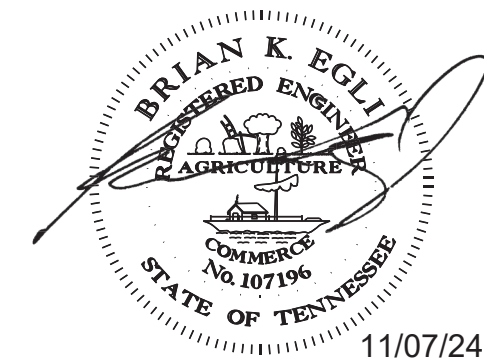
(LOOKING AHEAD ON SURVEY)



PHASE II CONSTRUCTION

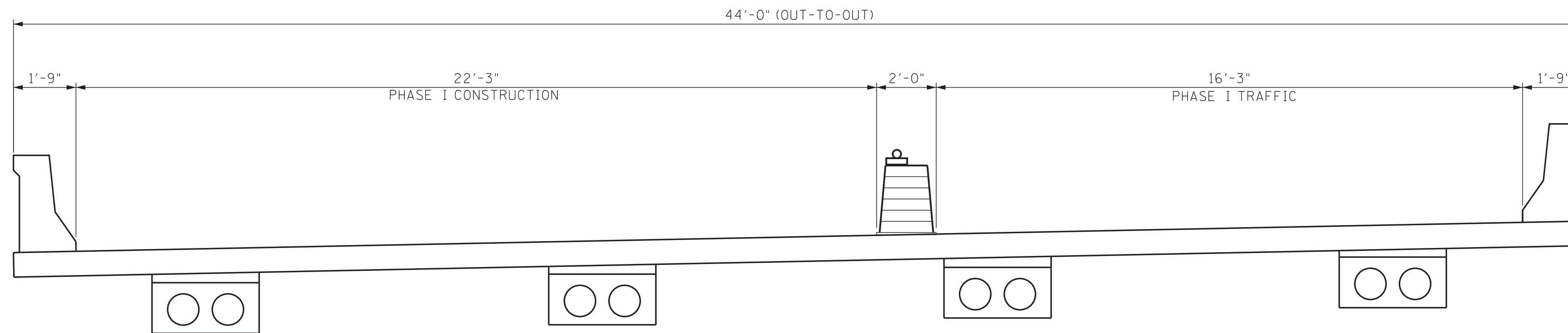
(LOOKING AHEAD ON SURVEY)

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 6/24
 SUPERVISED BY K. MARTINKO DATE 6/24
 CHECKED BY _____ DATE _____



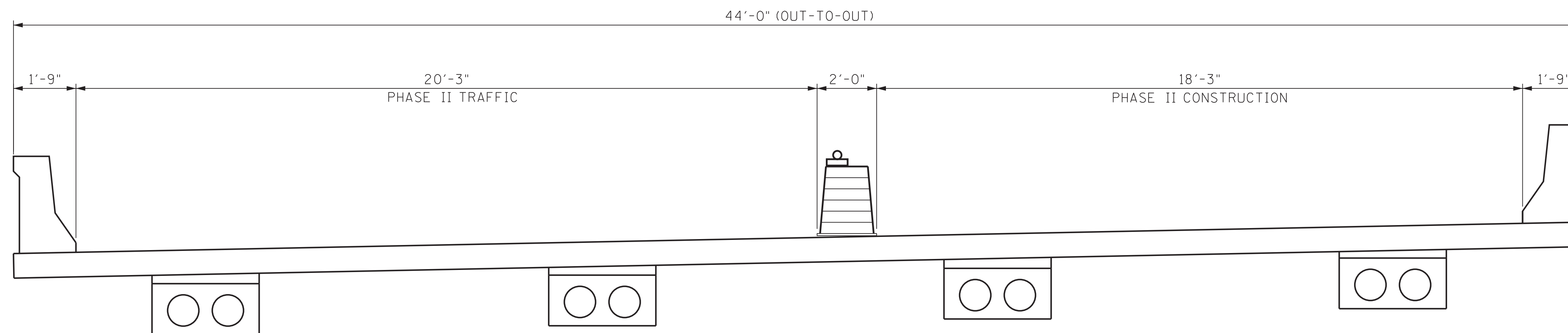
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
33-SR29-23.71R
OVER
BIG POSSUM CREEK
BR. NO. 33SR0290033
HAMILTON COUNTY
2025

PROJECT NO.	YEAR	SHEET NO.
33S029-M3-006	2025	B7

[illegible]

PHASE I CONSTRUCTION

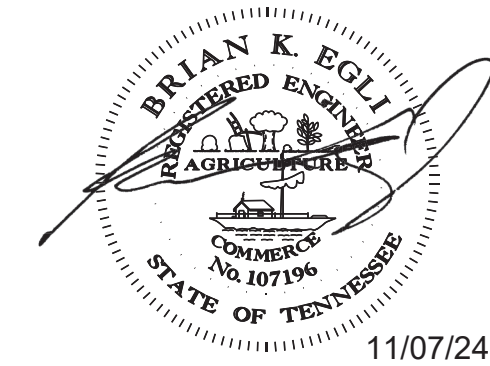
(LOOKING AHEAD ON SURVEY)



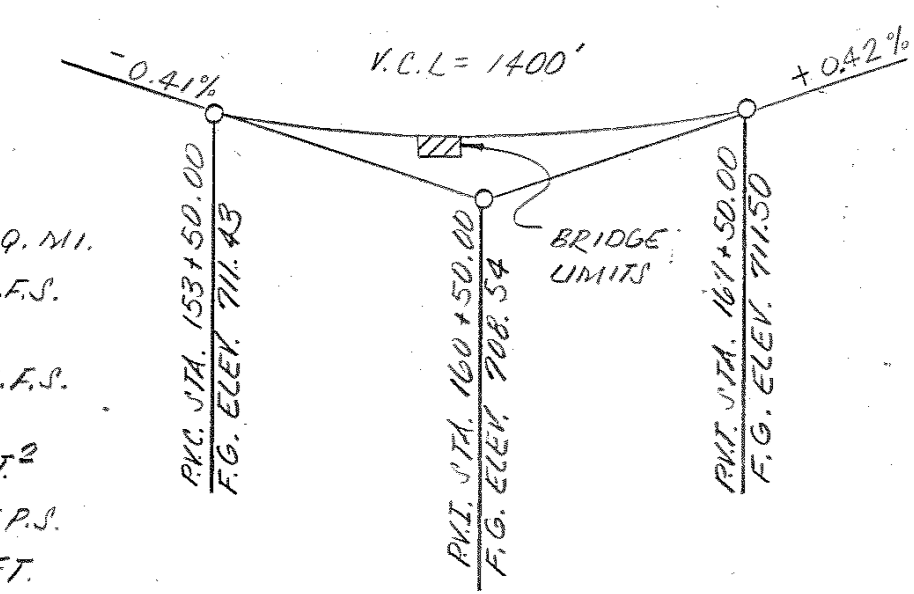
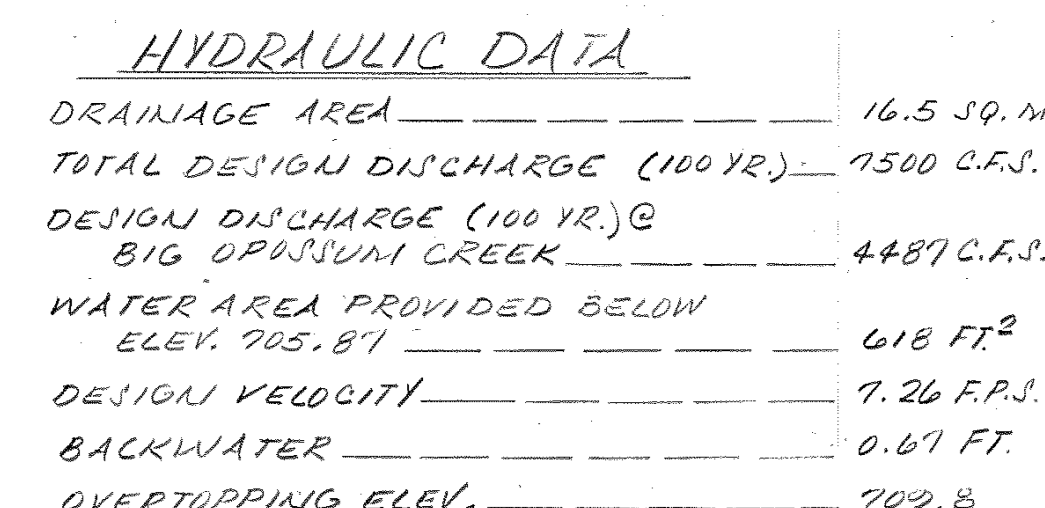
PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 6/24
 SUPERVISED BY K. MARTINKO DATE 6/24
 CHECKED BY _____ DATE _____



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
33-SR29-23.71L
OVER
BIG POSSUM CREEK
BR. NO. 33SR0290034
HAMILTON COUNTY
2025

[illegible]

<u>LIST OF DRAWINGS</u>	<u>DWG.NO.</u>	<u>LAST REV. DATE</u>
LAYOUT OF BRIDGE	M-136-110	3-10-92
GENERAL NOTES AND ESTIMATED QUANTITIES	M-136-111	3-10-92
FOUNDATION DATA	M-136-112	
SUPERSTRUCTURE	M-136-113	
SUPERSTRUCTURE DETAILS	M-136-114	
PRESTRESSED BOX-BEAM DETAILS	M-136-115	
ABUT. NO. 1 (LT. LANE) & ABUT. NO. 2 (RT. LANE)	M-136-116	
ABUT. NO. 2 (LT. LANE) & ABUT. NO. 1 (RT. LANE)	M-136-117	
PIERS NO. 1 & 2 (LT. & RT. LANES)	M-136-118	3-10-92
BILL OF STEEL	M-136-119	3-10-92
BILL OF STEEL	M-136-120	

<u>LIST OF STANDARD DWGS.</u>	<u>DWG. NO.</u>	<u>LAST REV DATE</u>
REINF. BAR SUPPORT DETAIL FOR CONC. SLAB	K-80-14	08-21-76
MINC. ABUT. & DRAINAGE DETAILS	K-85-150	06-25-87
STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	M-164-24	12-18-89
STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	M-164-25	12-18-89
TENN. STD. PRECAST PRESTRESSED DECK PANELS CONSTRUCTION DETAILS	M-164-25A	12-18-89
* STD. PILE DETAILS	M-174-150	
* BRIDGE RAILING CONCRETE PARAPET	M-233-1	
* REINF. CONC. PAVEMENT AT BRIDGE ENDS	M-233-2	
* BRIDGE END DRAIN DETAILS	M-233-3	
* BRIDGE END DRAIN DETAILS	M-233-4	
* BRIDGE END DRAIN DETAILS	M-233-5	
STD. CONC. MED. BARRIER	J-118-1	09-05-87

<u>LIST OF SPECIAL PROVISION</u>	<u>PROV. NO.</u>	<u>LAST REV. DATE</u>
REG. APPROVAL OF SHOP DRAWINGS	105A	07-21-84
REG. CONCRETE STRUCTURES	604	05-14-90
REG. STRUCTURAL CONCRETE	604C	08-01-89
REG. PRECAST, PRESTRESSED BRIDGE DECK PANELS	604P	05-08-89
REG. RIDEABILITY OF BRIDGE DECKS AND RDWY. APPROACHES	604R	5-14-90
REG. PRECAST, PRESTRESSED CONC. BRIDGE MEMBERS.	615	12-11-89
REG. EPOXY COATED REIN- FORCING STEEL	907A	03-25-85

A.D.T. 2010 = 17,000
42'0" RDWY. WITH 21-233-1
BRIDGE RAIL (BOTH BRIDGES)
DESIGN SPEED = 60 M.P.H.
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 1
LAYOUT OF BRIDGE
STATE ROUTE 29 (U.S. 27) OVER
BIG OPOSSUM CREEK (MAIN CHANNEL)
BRIDGE ID. NO. 33SR0290033
STATION 159+20.00
HAMILTON COUNTY
1990

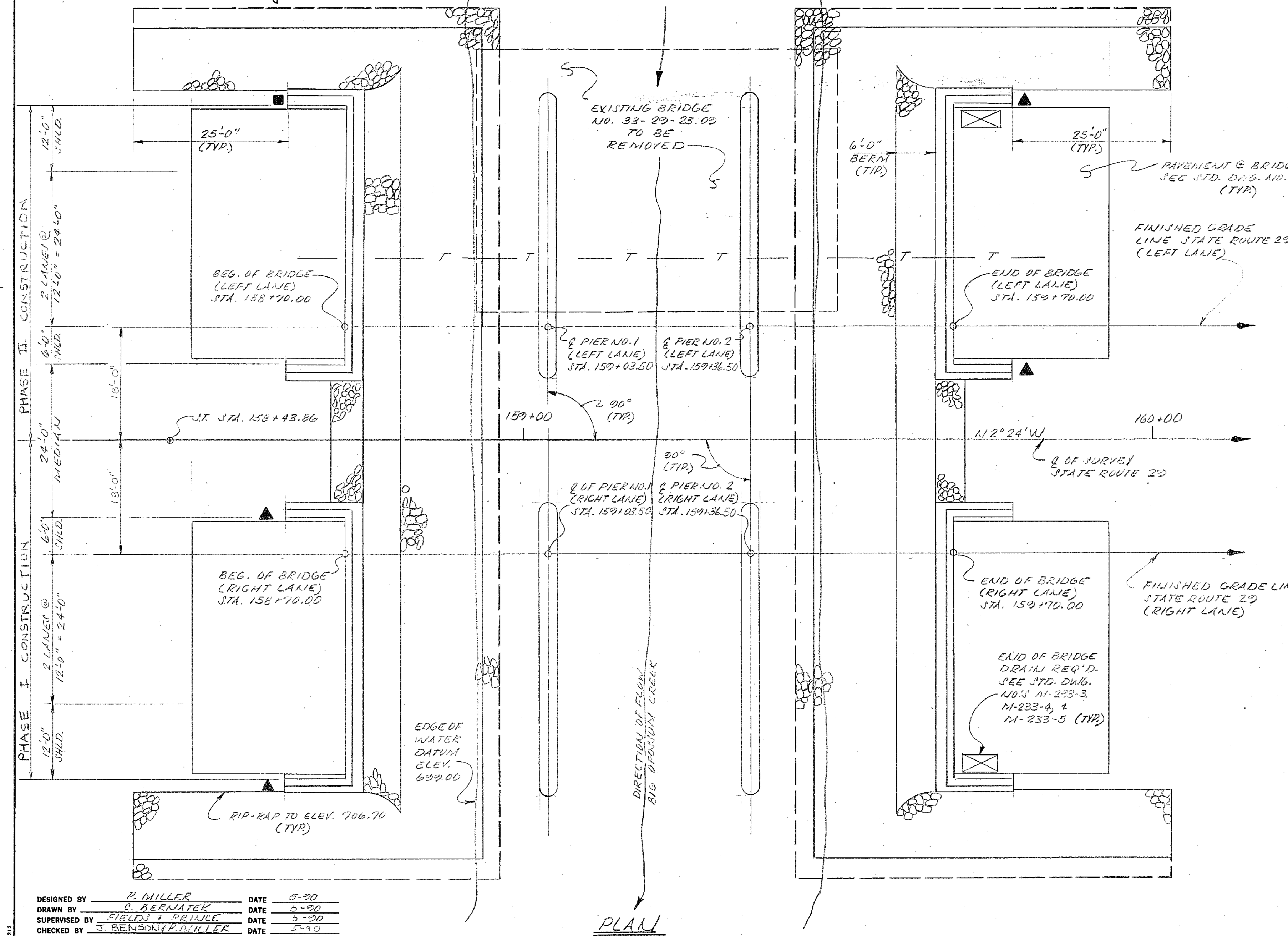
1950

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

APPROVED _____
DIRECTOR OF HIGHWAYS

14-136-110

CLASS "A" GRADING "D" 24 C.Y.



DESIGNED BY P. MILLER DATE 5-90
 DRAWN BY C. BERNATEK DATE 5-90
 SUPERVISED BY FIELDS & PRINCE DATE 5-90
 CHECKED BY J. BENSON & P. MILLER DATE 5-90

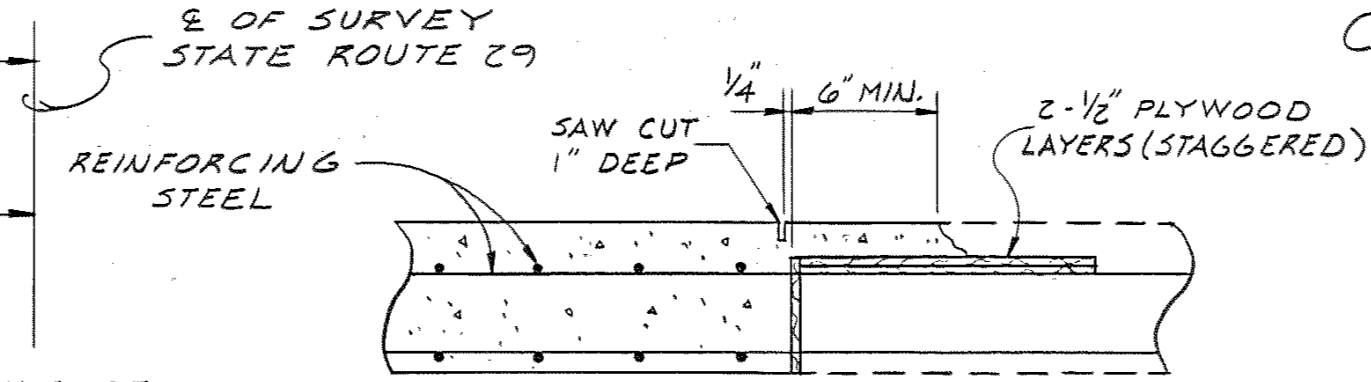
PLAN

CONST. NO. 33036-3264-04

PROJECT NO.	YEAR	SHEET NO.
SP	1990	

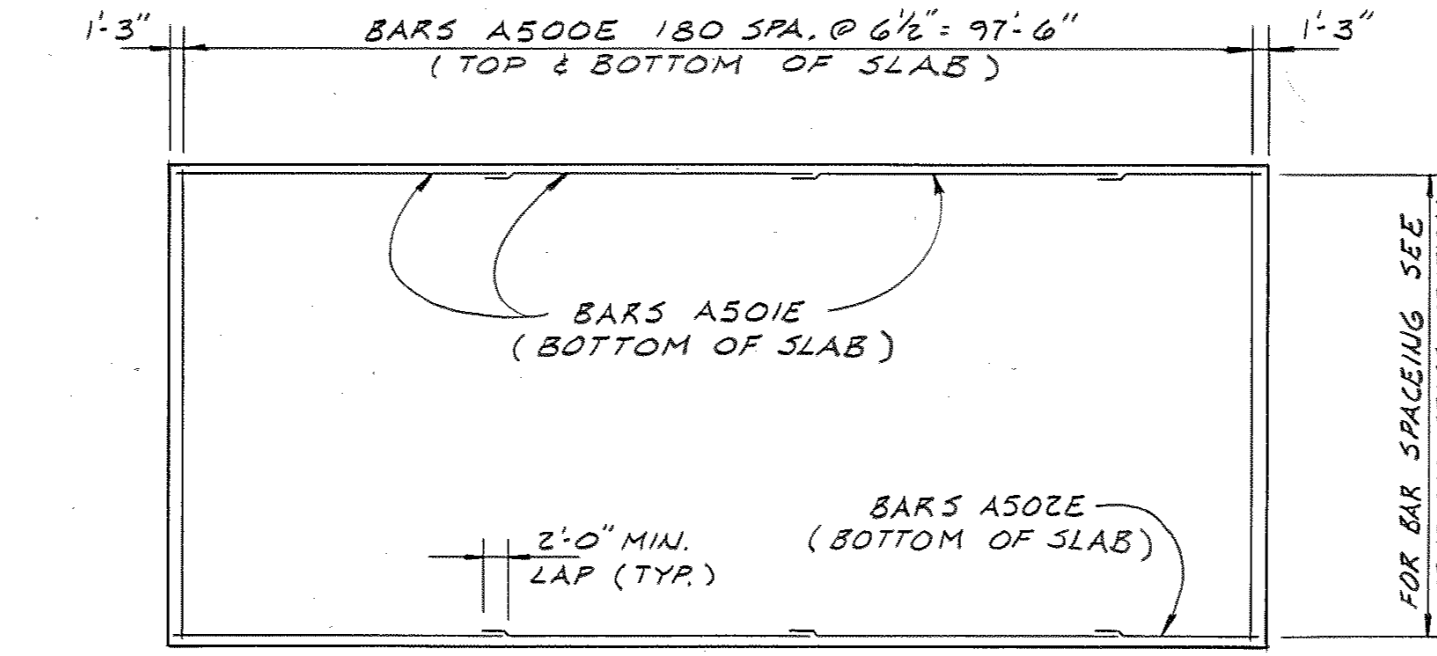
REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



SLAB CONSTRUCTION JOINT DETAIL

NOTE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION EXCEPT NO JOINT MAY BE LOCATED CLOSER THAN 1/5 SPAN LENGTH FROM AN INTERIOR SUPPORT. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT THE EXTERIOR BEAM FROM TWISTING. NO EQUIPMENT SHALL BE PERMITTED ON THE BRIDGE UNTIL ALL POURS ARE MADE AND THE CONCRETE IS PROPERLY CURED. ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN ABOVE.



SLAB PLAN

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

■ DENOTES: THESE QUANTITIES ARE PER LANE.

ESTIMATED QUANTITIES

CLASS "A" CONCRETE (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING LB.	CLASS "A" CONCRETE C.Y.	REINFORCING STEEL LB.
123	30,797	14	1,772

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BRIDGE No. 1
SUPERSTRUCTURE
LEFT AND RIGHT LANES
STATE ROUTE 29 (U.S. 27) OVER
BIG OPOSSUM CREEK (MAIN CHANNEL)
STATION 159+20.00
HAMILTON COUNTY

CORRECT *Edward P. Wasserman* 1990
ENGINEER OF STRUCTURES

APPROVED _____
DIRECTOR OF HIGHWAYS

11-136-113

